

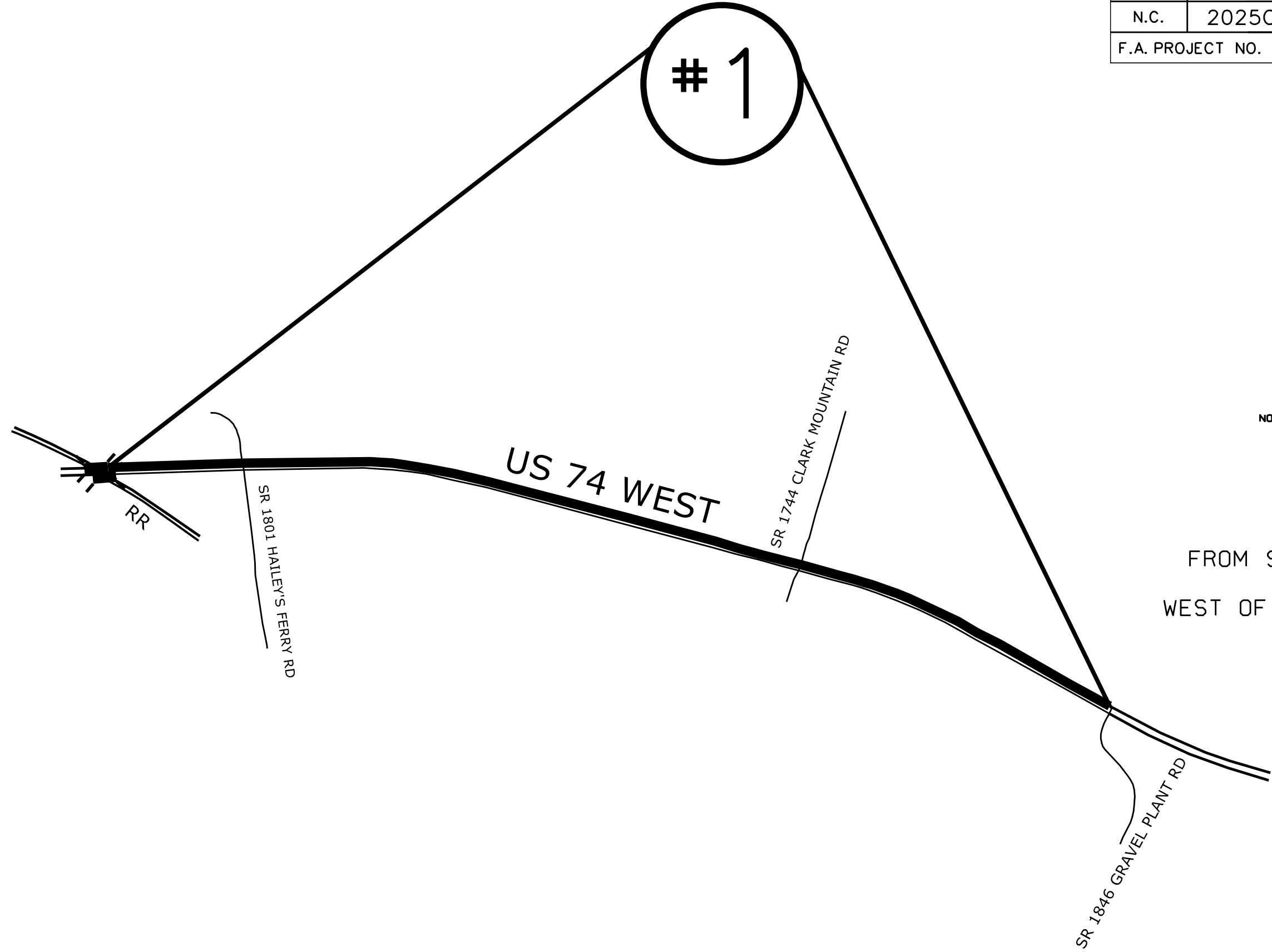
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	1	
F.A. PROJECT NO.			

1



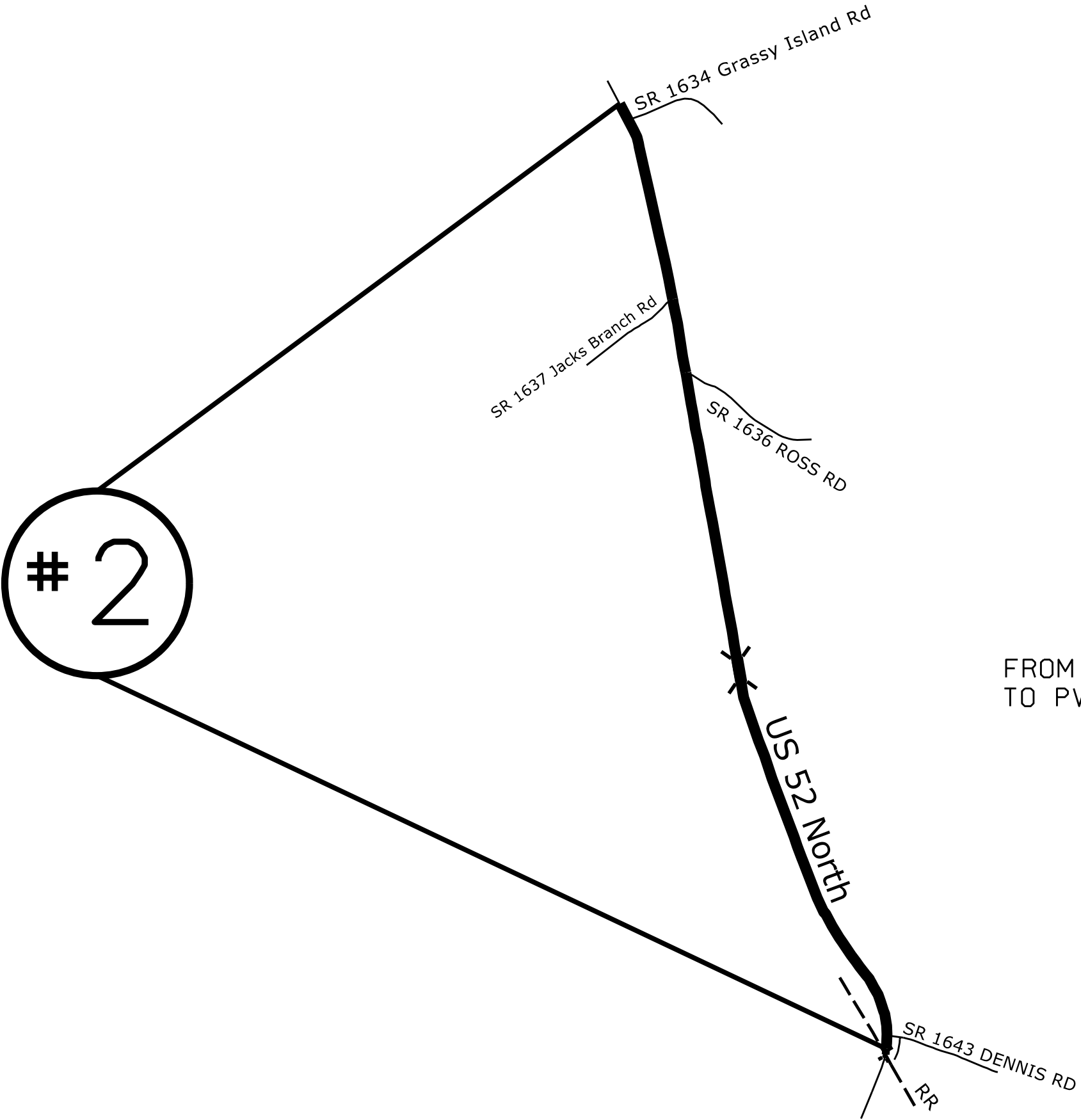
ENLARGED MUNICIPAL AND SUBURBAN AREAS
ANSON COUNTY
 NORTH CAROLINA
PREPARED BY THE
 NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #1 US 74 WEST
 2.1 MILES
 FROM SR 1846 GRAVEL PLANT RD
 TO BRIDGE JOINT
 WEST OF SR 1801 HAILEY'S FERRY RD



STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	2	
F.A. PROJECT NO.			

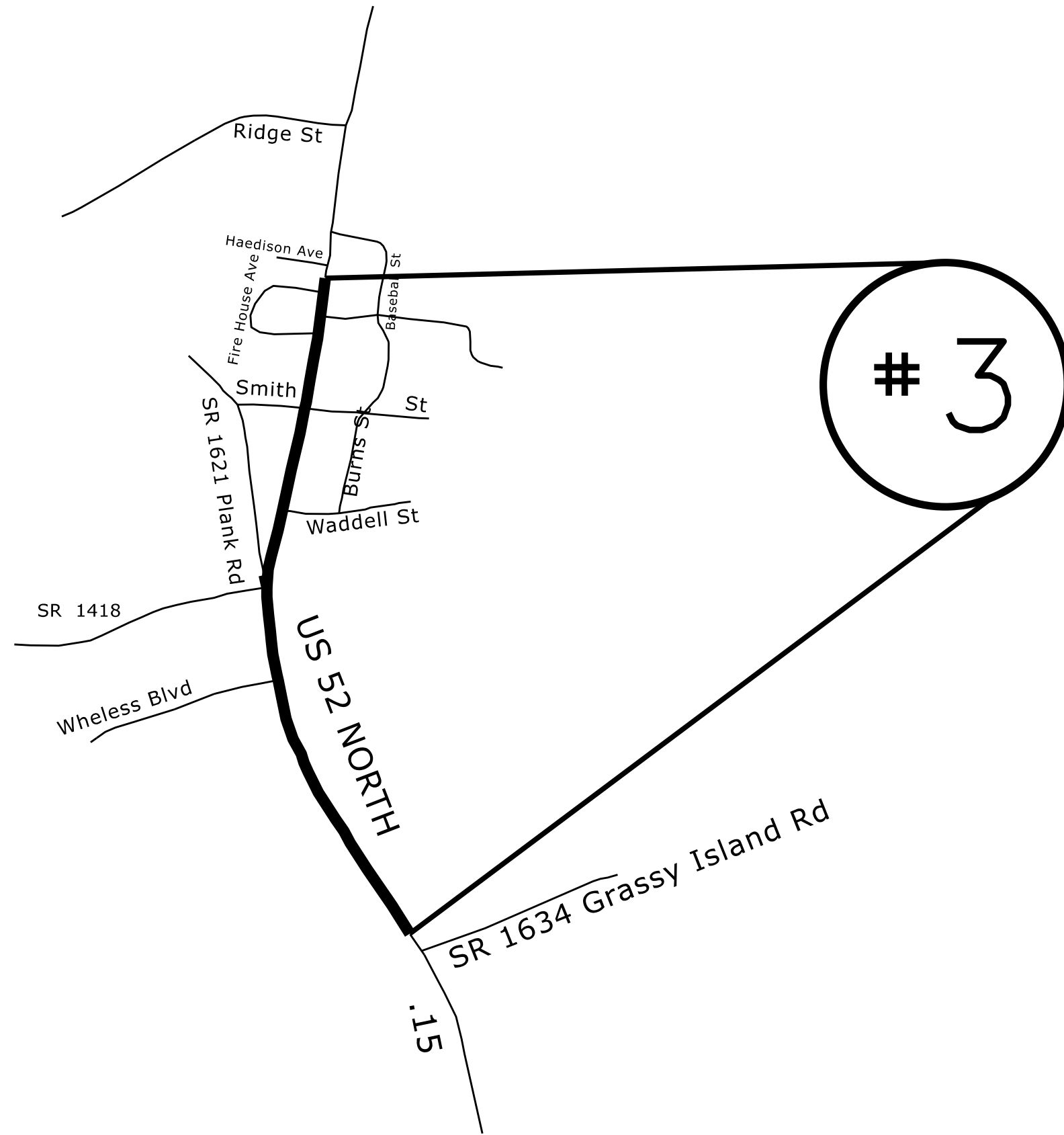
2



ENLARGED MUNICIPAL AND SUBURBAN AREAS
ANSON COUNTY
 NORTH CAROLINA
PREPARED BY THE
 NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #2 US 52 NORTH
 3.6 MILES
 FROM RAIL ROAD BRIDGE AT SR 1643 DENNIS RD
 TO PVMT JOINT PAST SR 1634 GRASSY ISLAND RD

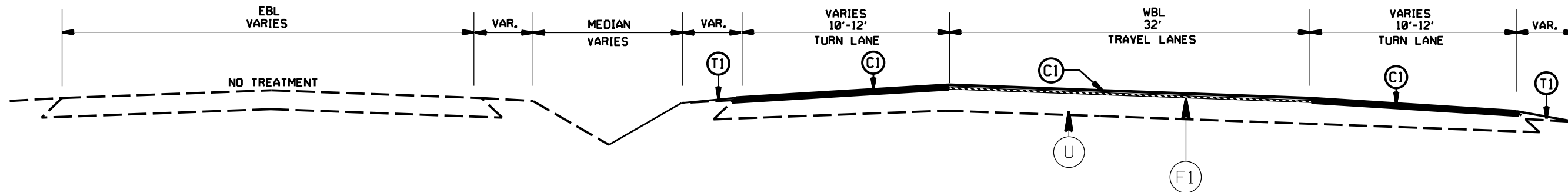
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	3	
F.A. PROJECT NO.			



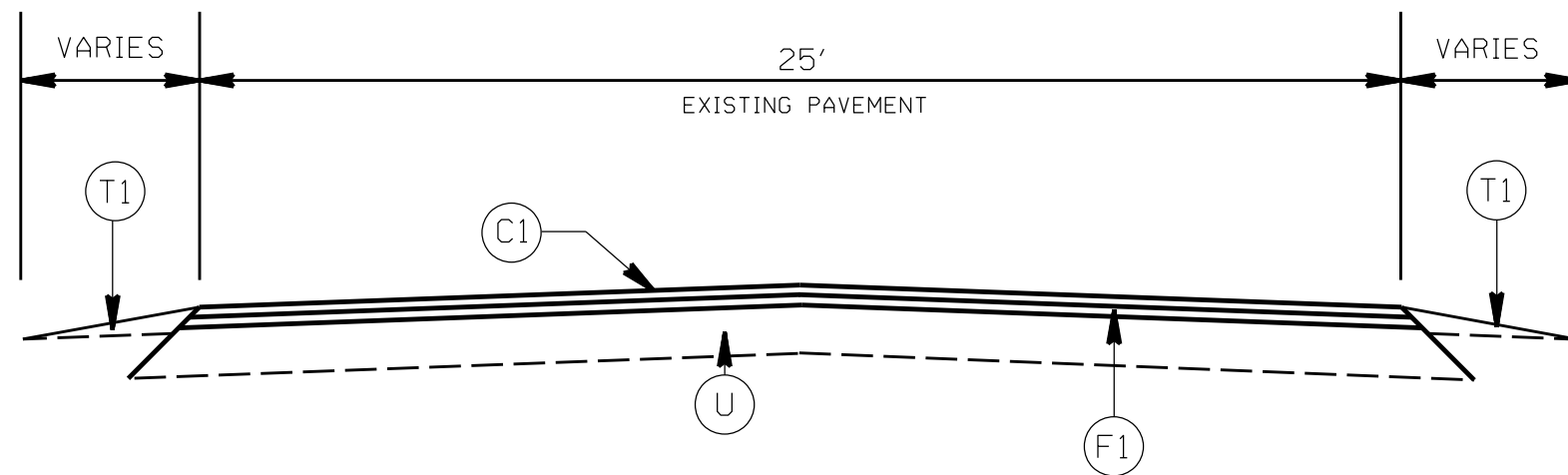
ENLARGED MUNICIPAL AND SUBURBAN AREAS
ANSON COUNTY
 NORTH CAROLINA
PREPARED BY THE
 NORTH CAROLINA DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS - DIVISION 10 DISTRICT 3

MAP #3 US 52 NORTH
 .69 MILES
 FROM PVMT JOINT SR 1634 GRASSY ISLAND RD
 TO PVMT JOINT PAST FIREHOUSE AVE

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	4	
F.A. PROJECT NO.			



TYPICAL SECTION NO. 1
US 74 WEST BOUND LANES (MAP 1)



TYPICAL SECTION NO. 2
US 52 NORTH (MAP 2)
APPROX STA. 10+00 TO 88+67
APPROX STA. 91+52 TO 200+08

PAVEMENT SCHEDULE

(C1)	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
(F1)	ASPHALT SURFACE TREATMENT, MAT COAT = 78M (SEE SPECIAL PROVISIONS)
(T1)	SHOULDER RECONSTRUCTION
(U)	EXISTING PAVEMENT
(V1)	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.

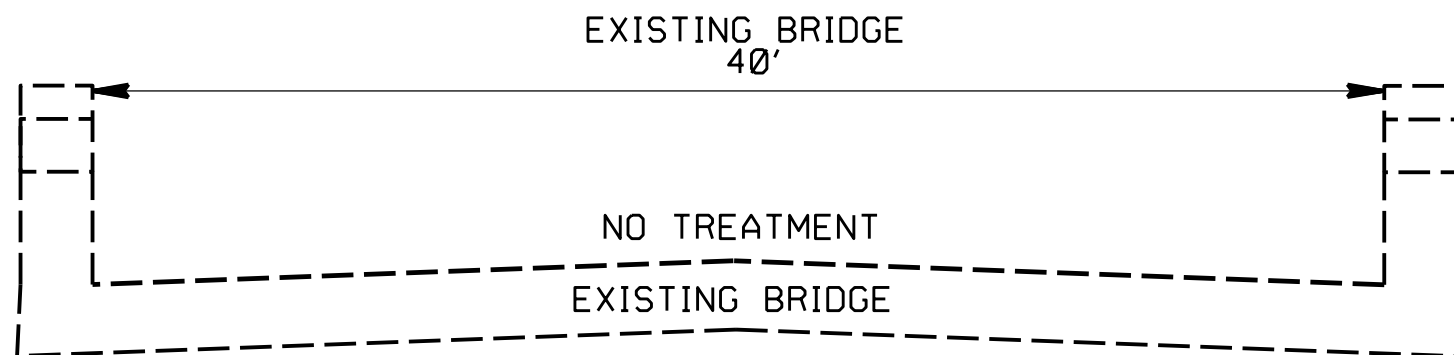
2025
ANSON COUNTY RESURFACING

SCALE	-NA-
DATE	7/21
DWG. BY	AWO
DESIGN BY	AWO
APPROVED	

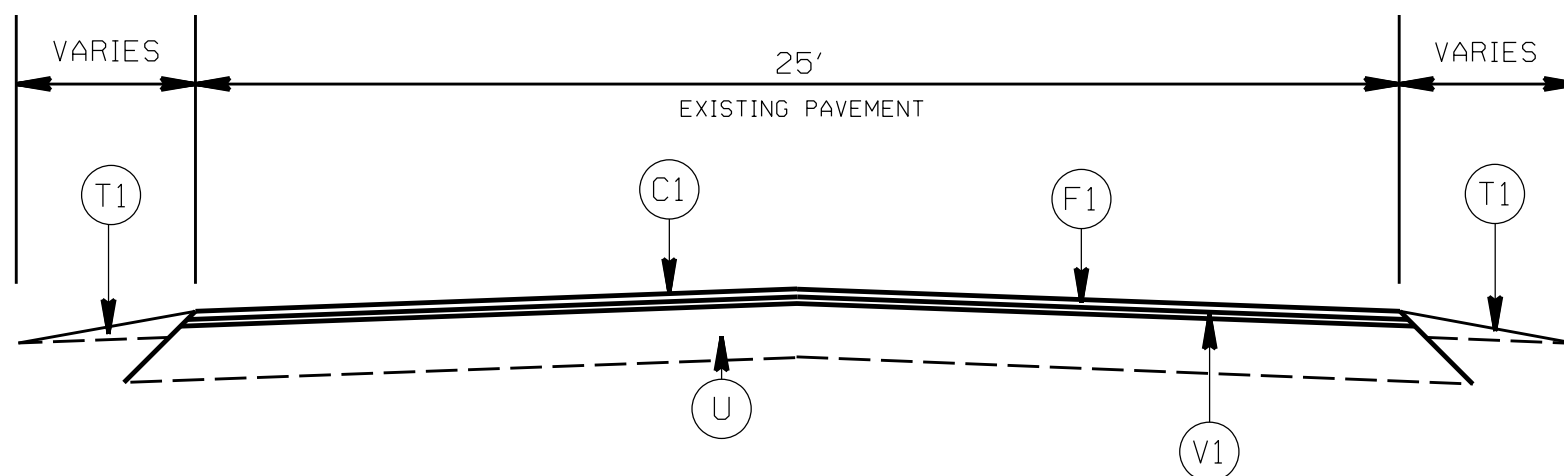


REVISIONS	

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	5	
F.A. PROJECT NO.			



TYPICAL SECTION NO. 3
 US 52 NORTH (MAP 2)
 APPROX STA. 88+67 TO 91+52




TYPICAL SECTION NO. 4
 US 52 NORTH (MAP 3)
 APPROX STA. 10+00 TO 20+03
 APPROX STA. 45+55 TO 46+79

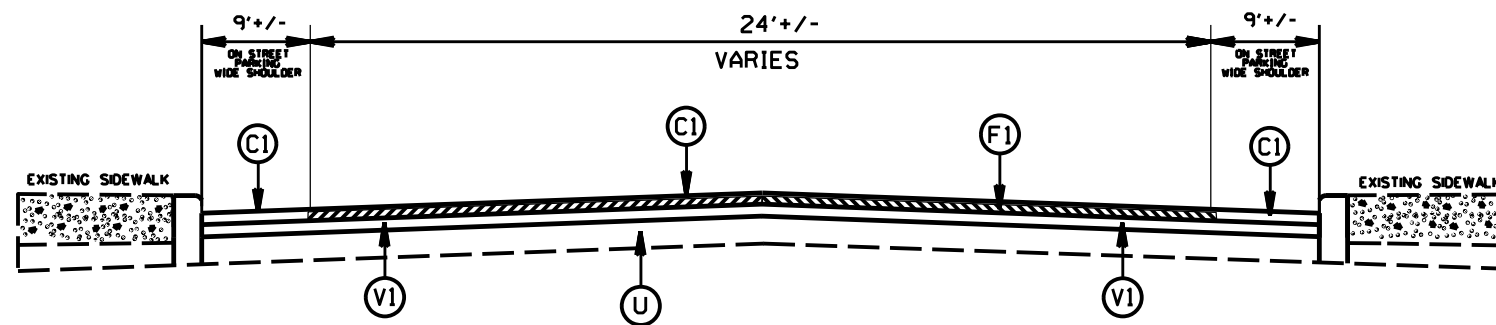
PAVEMENT SCHEDULE

(C1)	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
(F1)	ASPHALT SURFACE TREATMENT, MAT COAT * 78M (SEE SPECIAL PROVISIONS)
(T1)	SHOULDER RECONSTRUCTION
(U)	EXISTING PAVEMENT
(V1)	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.

2025
 ANSON COUNTY RESURFACING

SCALE	NA		REVISIONS
DATE	7/21		
DWG. BY	AMO		
DESIGN BY	AMO		
APPROVED			

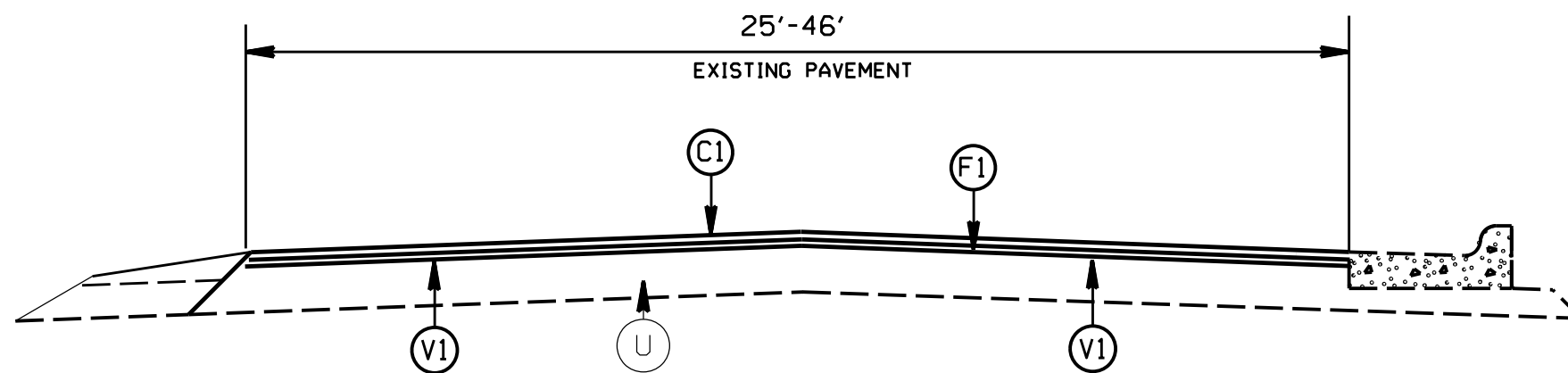
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	6	
F.A. PROJECT NO.			



TYPICAL SECTION NO. 5
 US 52 NORTH (MAP 3)
 APPROX STA. 20+03 TO 39+87

PAVEMENT SCHEDULE

(C1)	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
(F1)	ASPHALT SURFACE TREATMENT, MAT COAT * 78M (SEE SPECIAL PROVISIONS)
(T1)	SHOULDER RECONSTRUCTION
(U)	EXISTING PAVEMENT
(V1)	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.

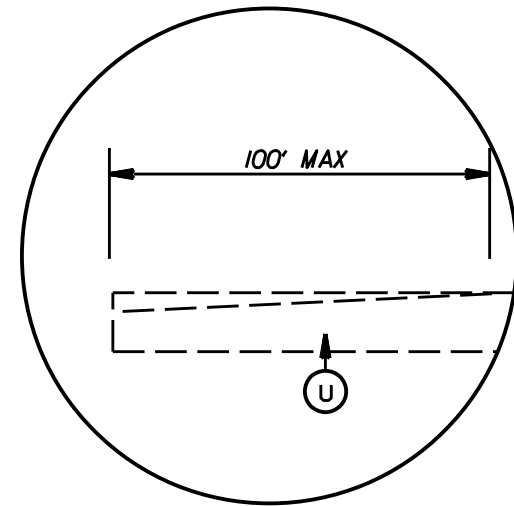


TYPICAL SECTION NO. 6
 US 52 NORTH (MAP 3)
 APPROX STA. 39+87 TO 45+55

2025
 ANSON COUNTY RESURFACING

SCALE	-1A-		REVISIONS	
DATE	7/21			
DWG. BY	AMO			
DESIGN BY	AMO			
APPROVED				

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	7	
F.A. PROJECT NO.			

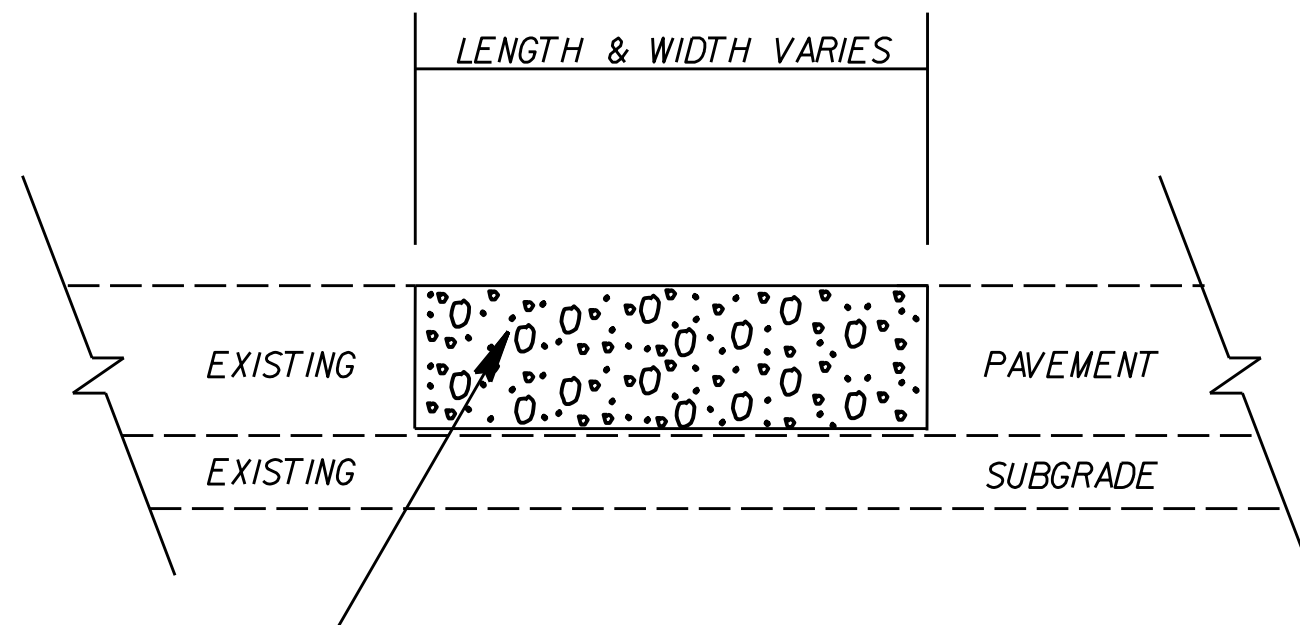


DETAIL FOR INCIDENTAL MILLING (0" TO 2.0")

PAVEMENT SCHEDULE

(C1)	PROP. APPROX. 2.0" ASPHALT CONC. SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 224 LBS. PER SQ. YD.
(F1)	ASPHALT SURFACE TREATMENT, MAT COAT ■ 78M (SEE SPECIAL PROVISIONS)
(T1)	SHOULDER RECONSTRUCTION
(U)	EXISTING PAVEMENT
(V1)	MILLING OF EXISTING PAVEMENT, 2.0" DEPTH.

PATCHING DETAIL



RATE IS VARIABLE AND SHALL BE AS DIRECTED BY THE ENGINEER. ASPHALT TYPE 119.0C SHALL BE PLACED.

2025 ANSON COUNTY RESURFACING			
SCALE	-NA-		REVISIONS
DATE	7/21		
DWG. BY	AJB		
DESIGN BY	AJB		
APPROVED			

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	9	
F.A. PROJECT NO.			

PROJECT NOTES:

1. MAP 1, DO NOT PLACE #78M MAT COAT IN TURN LANES
2. MAP 2, NO TREATMENT WILL BE APPLIED TO THE EXISITNG BRIDGE APPROX STA. 86+67 TO 91+52.
3. MAP 3, DO NOT APPLY #78M MAT COAT TO THE WIDE SHOULDERS (ON STREET PARKING) APPROX STA. 20+03 TO 39+87.
4. MAPS 1 AND 2, DO NOT PLACE #78m MAT COST ON Y-LINES WHERE PAVING LIMITS HAVE BEEN EXTENDED BEYOND THE RADIUS.
5. DEPTH OF PATCHING WILL BE AS DIRECTED BY THE ENGINEER.
6. SHOULDER RECONSTRUCTION WILL BE AS DIRECTED BY THE ENGINEER.
7. THE CONTRACTOR WILL COORDINATE WORK ON MAP 3 TO MINIMIZE TRAFFIC IMPACTS THAT WOULD CONTRIBUTE TO ADDITIONAL CONJESTION AT DROPOFF AND PICKUP BELL TIMES FOR ANSONVILLE ELEMENTARY.

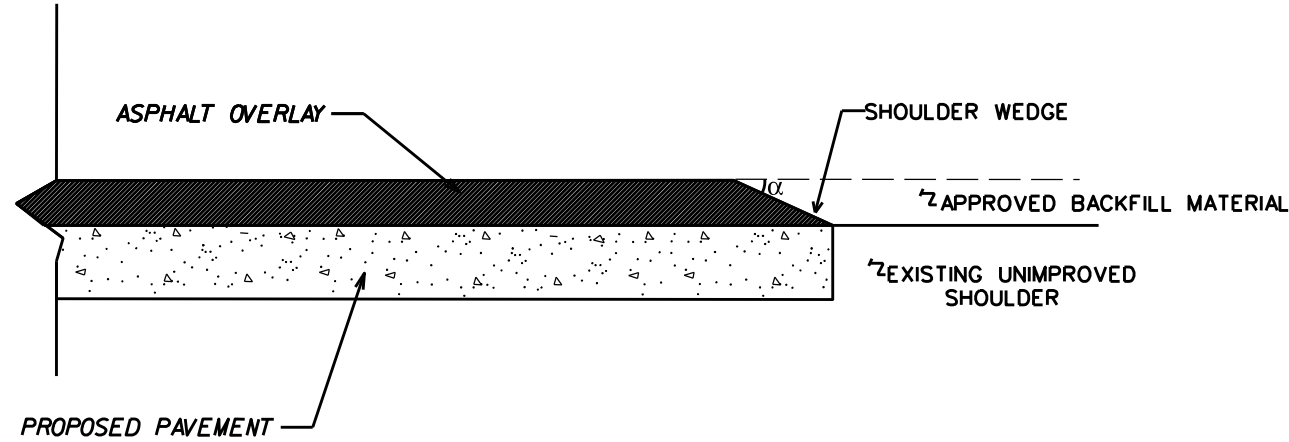
2025
ANSON COUNTY RESURFACING

SCALE	NA		REVISIONS	
DATE	7/21			
DWG. BY	AJB			
DESIGN BY	AJB			
APPROVED				

NOTES:

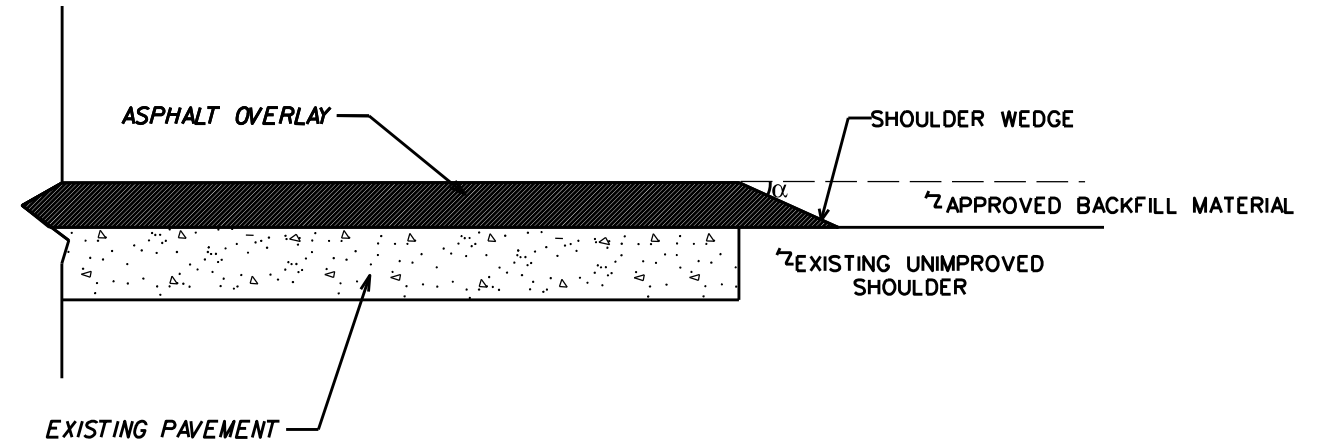
- 1) DETAIL DOES NOT APPLY TO OGAFCC AND ULTRA-THIN BONDED WEARING COURSE.
- 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
- 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	10	
F.A. PROJECT NO.			



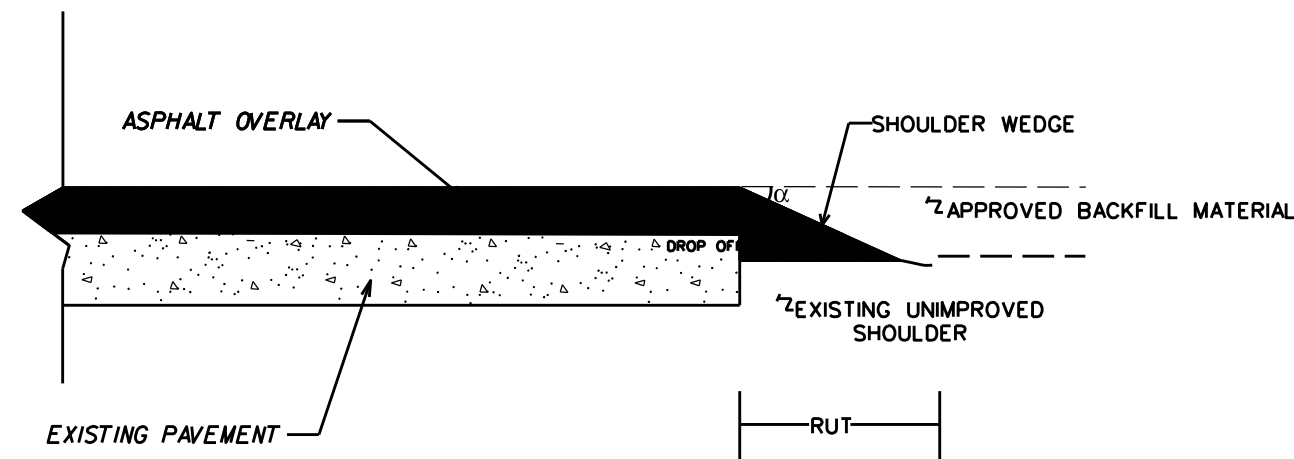
SHOULDER WEDGE DETAIL

(RESURFACING PROJECTS W/ WIDENING OR WITH EXISTING PAVED SHOULDER HAVING NO DROPOFFS)




SHOULDER WEDGE DETAIL

(RESURFACING PROJECTS W/ NO WIDENING)



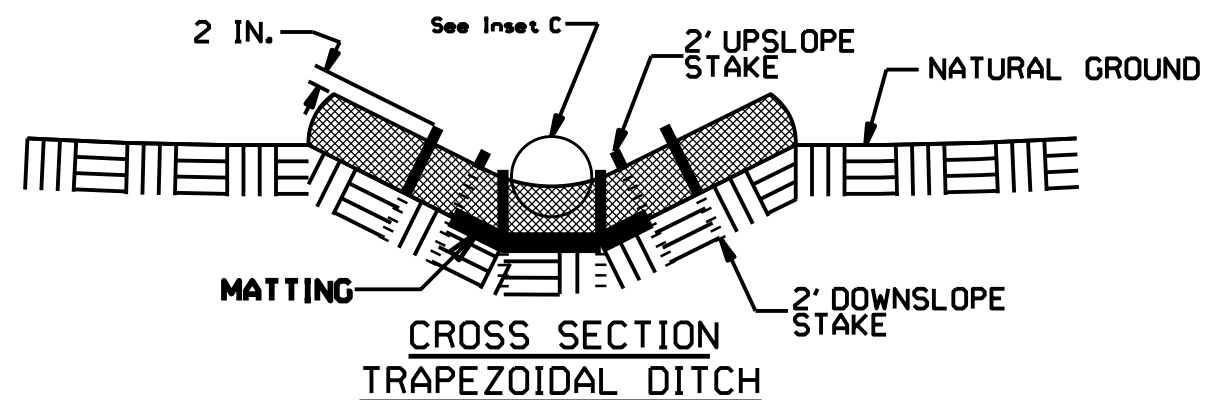
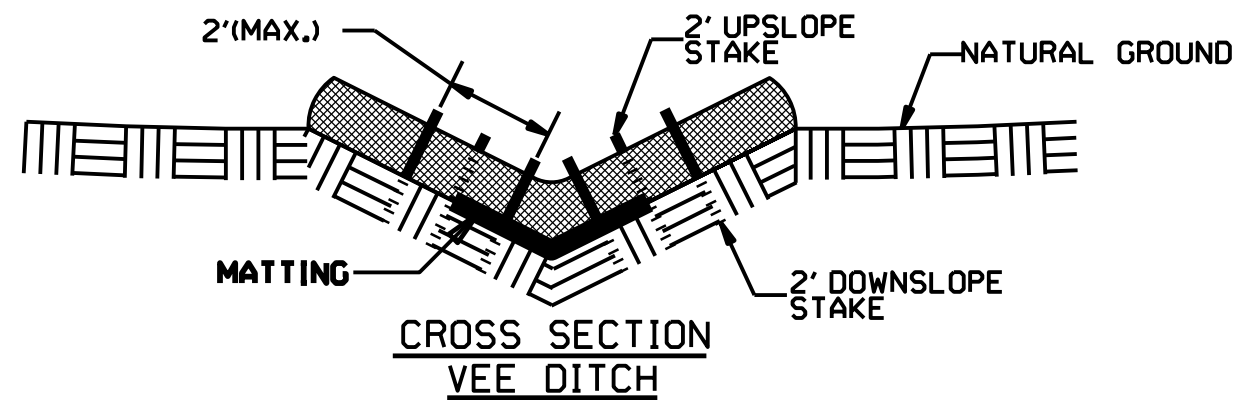
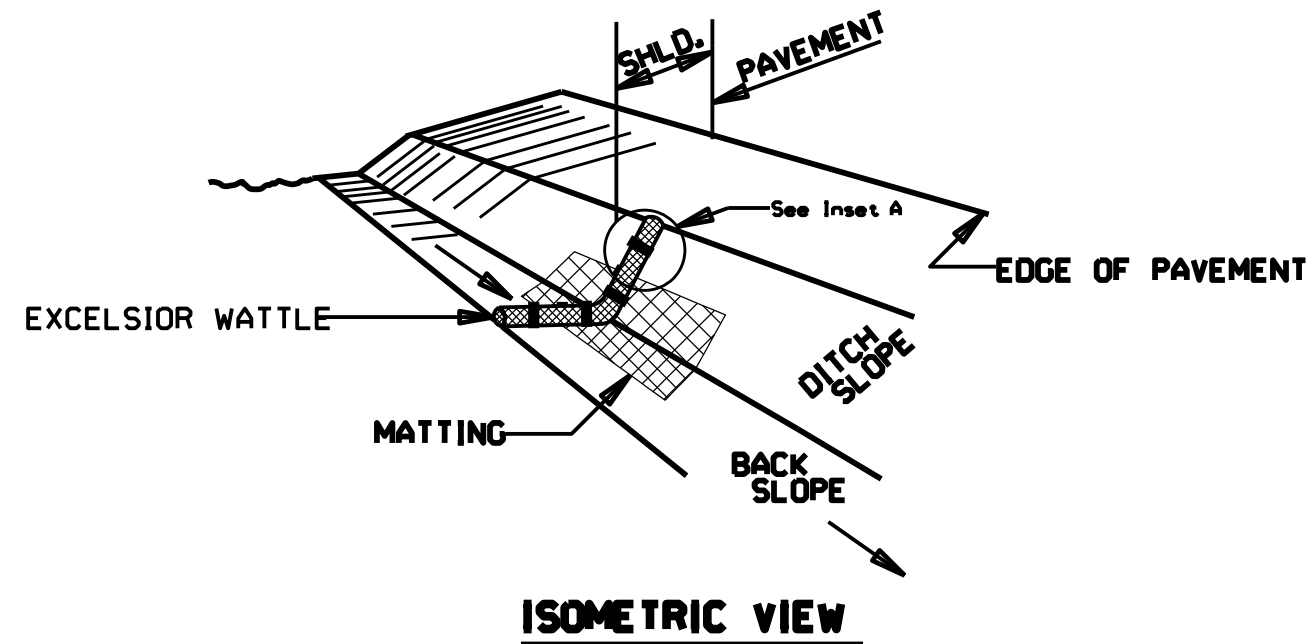
SHOULDER WEDGE DETAIL
(RESURFACING ADJACENT TO RUTTED SHOULDER)

$\alpha = 30$ DEGREES

SHOULDER WEDGE DETAILS		
SCALE	NA	
DATE	7/21	
DWG. BY	AJB	
DESIGN BY	AJB	
APPROVED		
		REVISIONS

WATTLE WITH POLYACRYLAMIDE DETAIL

STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	EC1	
F.A. PROJECT NO.			



NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. CROSS SECTION.

INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

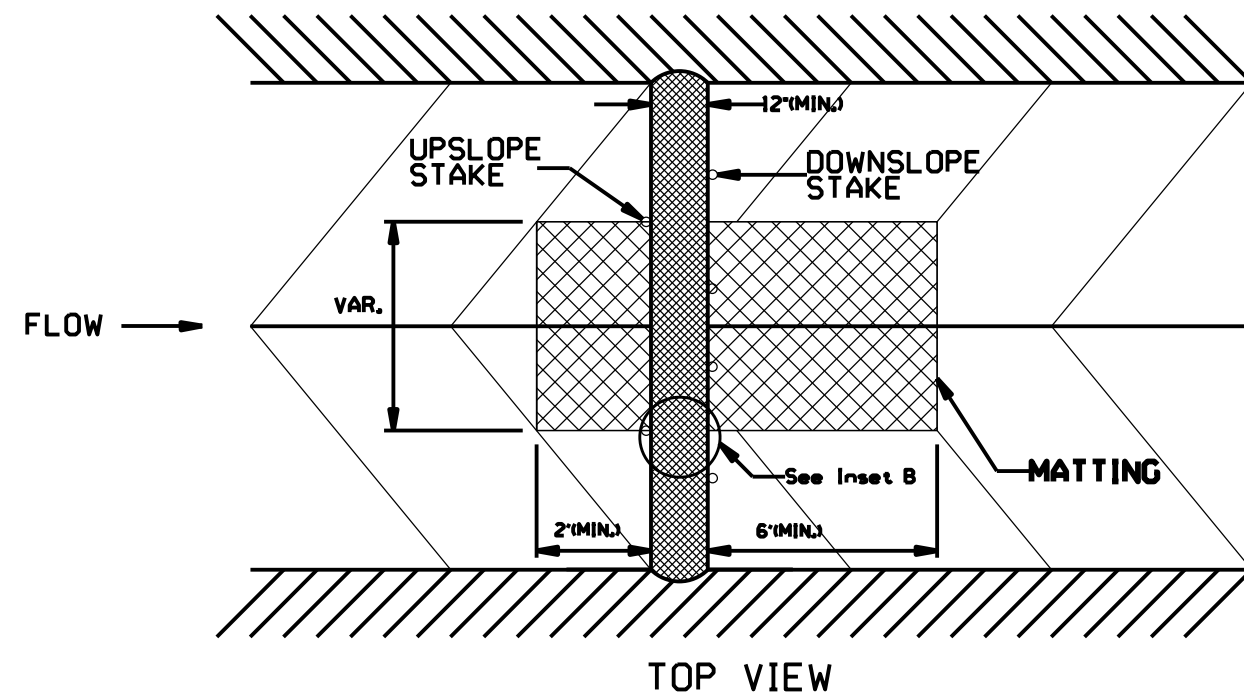
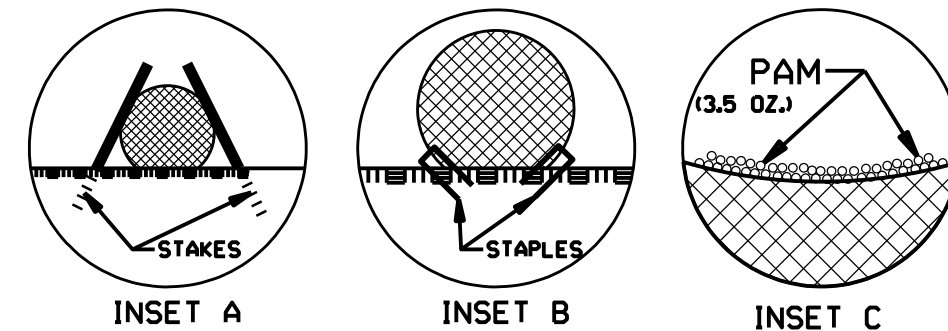
PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

PRIOR TO POLYACRYLAMIDE (PAM) APPLICATION, OBTAIN A SOIL SAMPLE FROM PROJECT LOCATION, AND FROM OFFSITE MATERIAL, AND ANALYZE FOR APPROPRIATE PAM FLOCCULANT TO BE APPLIED TO EACH WATTLE.

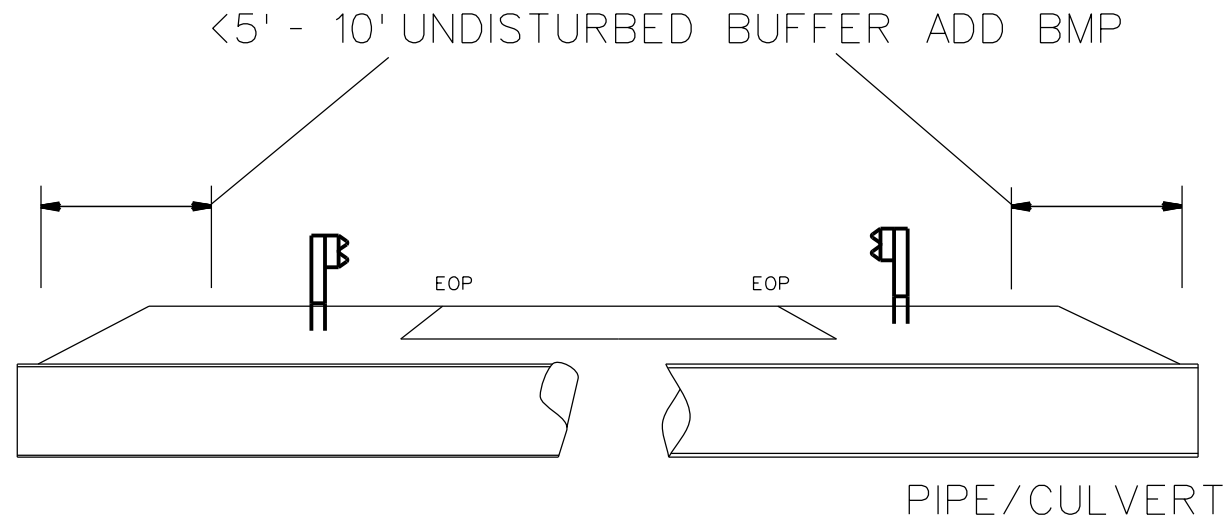
INITIALLY APPLY 3.5 OUNCES OF ANIONIC OR NEUTRALLY CHARGED POLYACRYLAMIDE (PAM) OVER WATTLE WHERE WATER WILL FLOW AND AFTER EVERY RAINFALL EVENT THAT IS EQUAL TO OR EXCEEDS 0.50 IN.



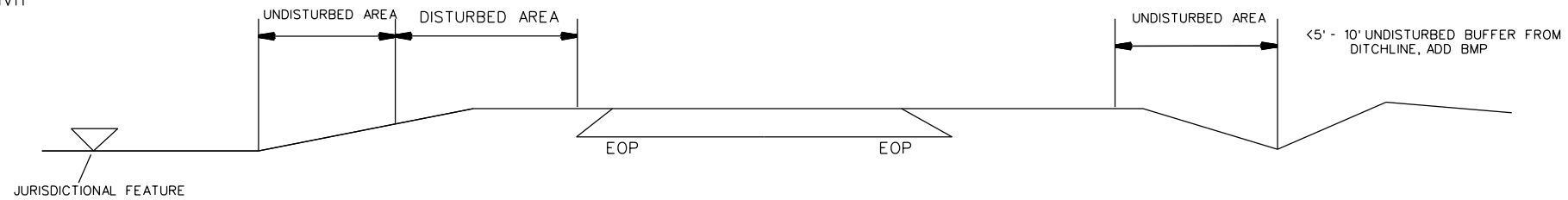
NOTES: LESS THAN 5' - 10' UNDISTURBED BUFFER FROM ROW, DITCHLINE, WATER FEATURE, OR DRAINAGE INLET, ADD BMP.

BMP OPTIONS: WATTLE OR SILT FENCE

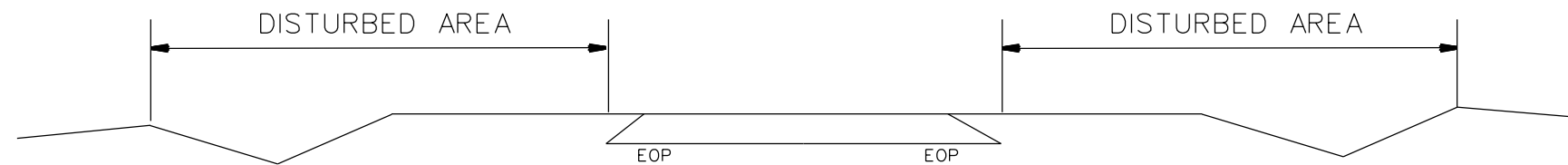
STATE	PROJECT NO.	SHEET NO.	TOTAL SHEETS
N.C.	2025CPT.10.02.10041	EC2	
F.A. PROJECT NO.			



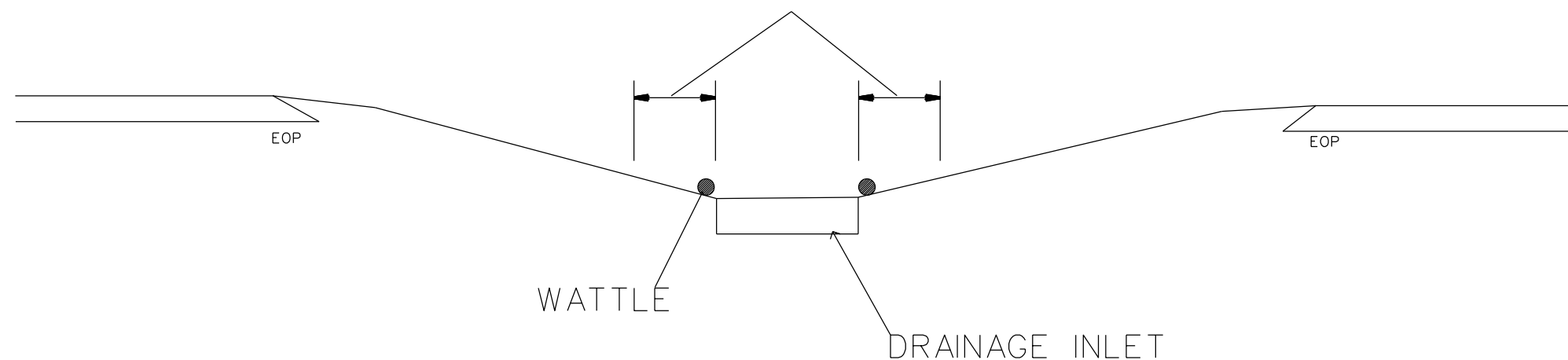
<5' - 10' UNDISTURBED BUFFER FROM JURISDICTIONAL FEATURE ADD BMP




USE BMP'S IF SHOULDERS AND/OR FRONTSLOPES AND/OR DITCHLINE AND/OR BACKSLOPES ARE DISTURBED

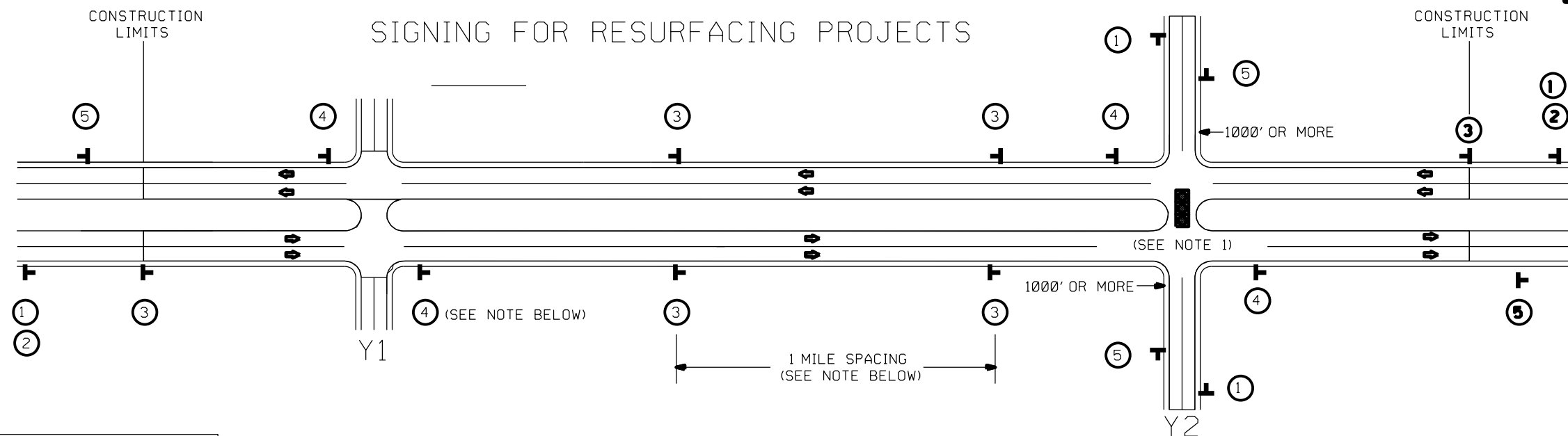


<5' - 10' UNDISTURBED BUFFER FROM INLET, ADD WATTLE



EROSION CONTROL DETAIL

SCALE	NA		REVISIONS
DATE	1/20		
DWG. BY	AJB		
DESIGN BY	AJB		
APPROVED			

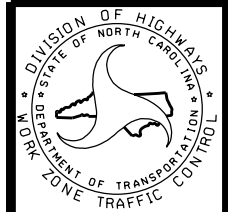


LEGEND	
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

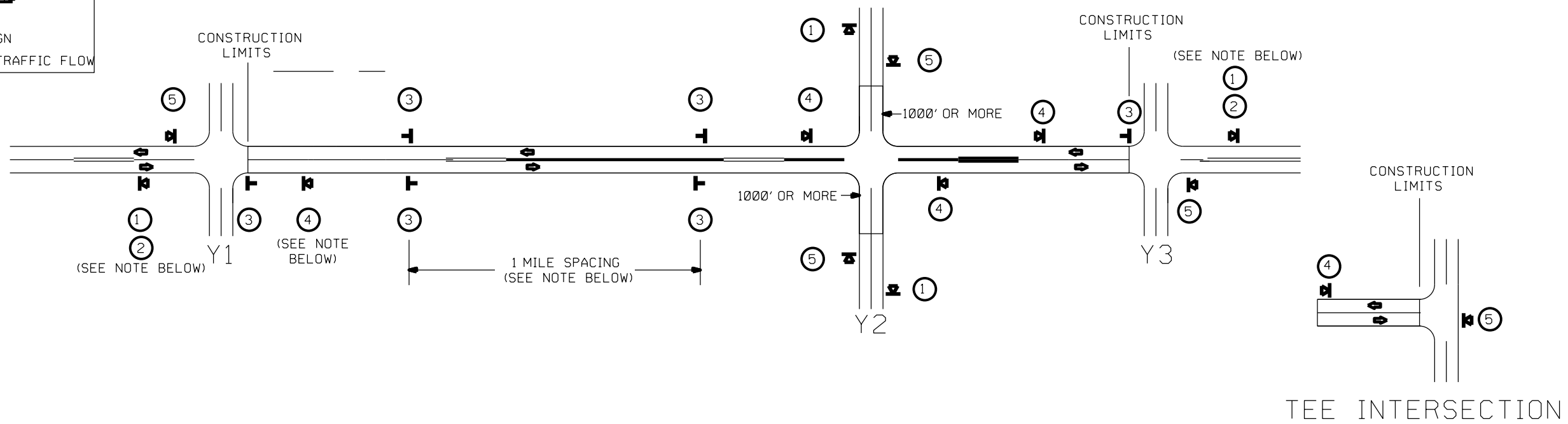
SIGNING NOTES AND PLACEMENT PER DIRECTION	<p>1 PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>2 #2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER.(NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <p> PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p> <p>NOTES:</p> <ol style="list-style-type: none"> 1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
	<p>3 PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	
	<p>4 THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	
	<p>5 PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>	



RESURFACING
ADVANCE WARNING SIGNS
FOR RURAL AND SUBURBAN
MULTI-LANE ROADWAYS
W/ SHOULDER SECTIONS

SIGNING FOR ASPHALT SURFACE TREATMENT

LEGEND	
	PORTABLE SIGN
	STATIONARY SIGN
	DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

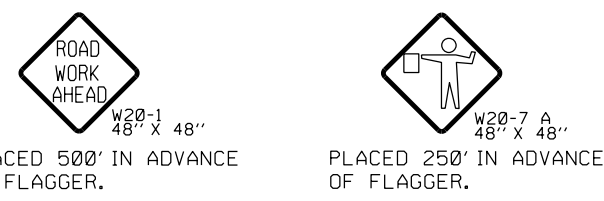
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	1	W20-1 48" X 48" W7-3aP 24" X 18" - PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. - SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS.
	2	
	3	W8-7 48" X 48" SP 48" X 48" - ALTERNATE THE FOLLOWING TWO SIGNS: - STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT". - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.
	4	SP 13106 48" X 48" - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.
	5	G20-2 A 48" X 24" PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.

STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:

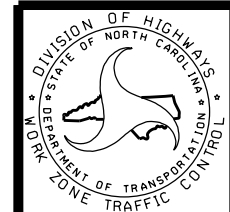
- 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
- 2) SUBDIVISION ROADS
- 3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.

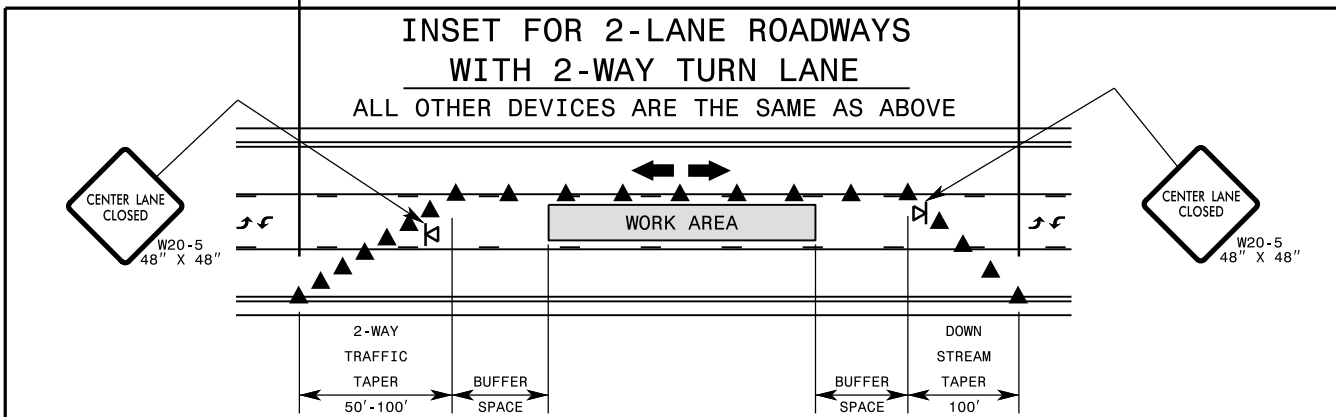
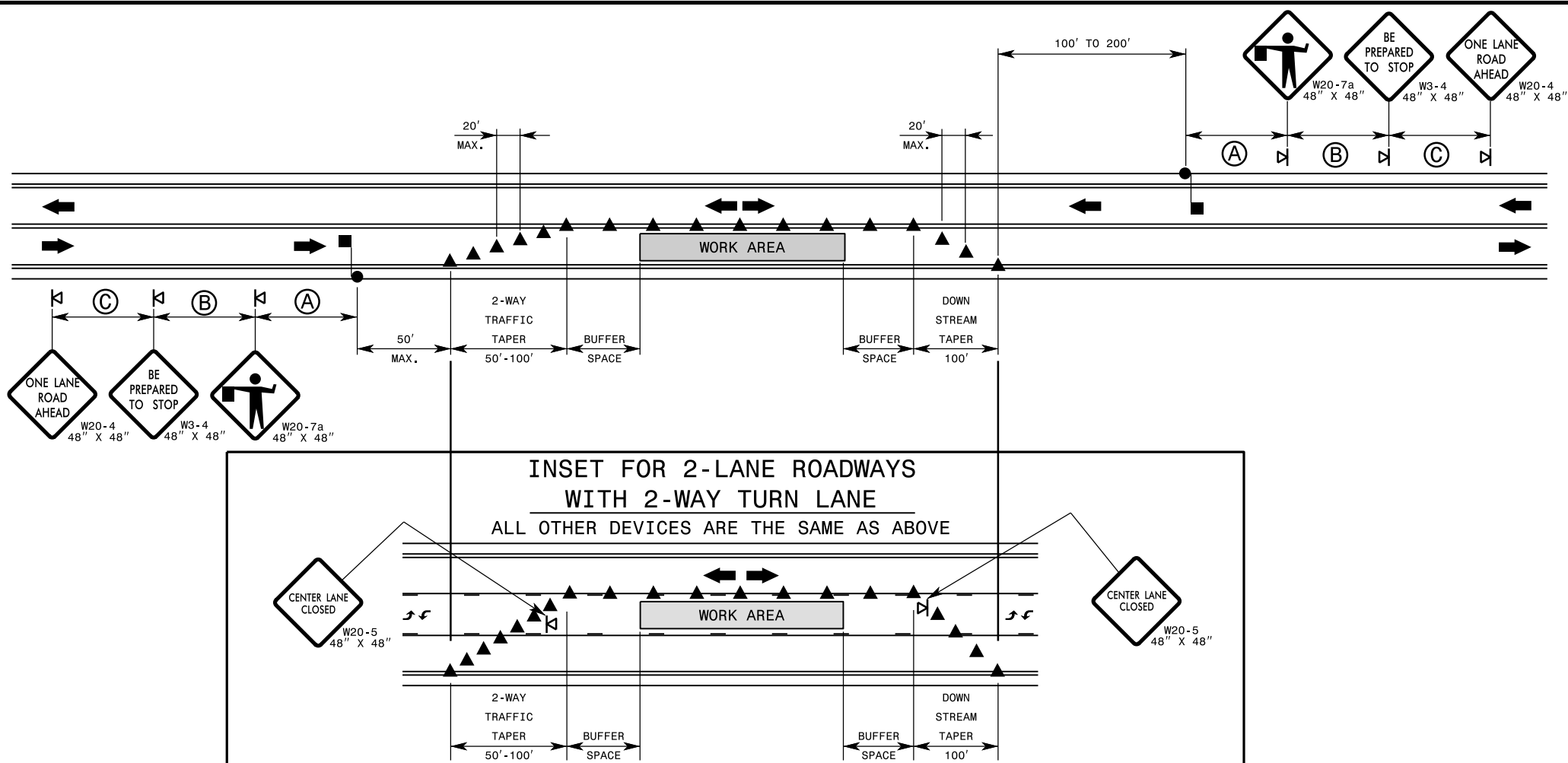


THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.

MAPS LESS THAN 2 MILES
FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.



ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT



GENERAL NOTES FOR FLAGGING OPERATIONS

- 1- REFER TO RSD. 1101.11, SHEETS 1 & 4, FOR "L" DISTANCE AND SIGN SPACING.
- 2- INSTALL LANE CLOSURES WITH THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE UPSTREAM SIDE OF TRAFFIC.
- 3- REMOVE LANE CLOSURES AGAINST THE TRAFFIC FLOW, BEGINNING WITH DEVICES ON THE DOWNSTREAM SIDE OF TRAFFIC.
- 4- FOR POSTED SPEED LIMITS BELOW 45 MPH, CHANNELIZING DEVICE SPACING SHALL BE 20 FEET IN THE TAPERS AND THE SHIFTS AND 40 FEET IN THE TANGENTS. FOR POSTED SPEED LIMITS GREATER THAN OR EQUAL TO 45 MPH, CHANNELIZING DEVICE SPACING SHALL BE 40 FEET IN THE TAPERS AND THE SHIFTS AND 80 FEET IN THE TANGENTS.
- 5- EXTEND LANE CLOSURES AT THE BUFFER SPACE SUCH THAT STOPPING SIGHT DISTANCE IS PROVIDED TO THE FLAGGER (REFER TO RSD. 1101.11, SHEET 2).
- 6- DO NOT STOP ALL DIRECTIONS OF TRAFFIC FOR MORE THAN 5 MINUTES AT A TIME.
- 7- DRUMS OR SKINNY DRUMS MAY BE USED IN LIEU OF CONES. REFER TO RSD. 1180.01 FOR SKINNY DRUM REQUIREMENTS.
- 8- USE FLAGGERS TO CONTROL TRAFFIC AT INTERSECTIONS AFFECTED BY THE LANE CLOSURE. SUPPLEMENT FLAGGERS LOCATED AT INTERSECTIONS WITH FLAGGER AHEAD SIGNS (W20-7A) PLACED APPROXIMATELY 250 FT. IN ADVANCE OF THE FLAGGER. FOR SIGNALIZED INTERSECTIONS PLACE SIGNALS IN THE FLASH MODE AND USE LAW ENFORCEMENT.
- 9- REFER TO THE CURRENT MUTCD FOR FLAGGER CONTROL, REQUIREMENTS, AND PROCEDURES.
- 10- DO NOT EXCEED A 1 MILE LANE CLOSURE LENGTH UNLESS OTHERWISE SHOWN IN THE TMP OR AS DIRECTED BY THE ENGINEER.

- 11- IF VEHICLE QUEUES WILL REACH WITHIN 100' OF EITHER SIDE OF ACTIVE RAILROAD TRACKS, PROVIDE A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER TO PREVENT VEHICLES FROM STOPPING WITHIN THE GRADE CROSSING. PROVIDE OFFICER OR FLAGGER EVEN IF AUTOMATIC WARNING MEASURES ALREADY EXIST.
- 12- THIS DETAIL IS APPLICABLE FOR OPERATIONS IN PLACE FOR 72 HOURS OR LESS. FOR LONGER DURATION OPERATIONS, SIGNING AND PAVEMENT MARKINGS MAY NEED TO BE ALTERED.

GENERAL NOTES FOR PILOT CAR OPERATIONS

- 1- USE PILOT CARS WHEN DIRECTED BY THE ENGINEER.
- 2- IF ROADWAY WIDTH IS LESS THAN 22 FEET (EOP TO EOP), CONES MAY NOT BE REQUIRED ALONG WORK AREA, AND AT THE DISCRETION OF THE ENGINEER, CONES MAY BE OMITTED ALONG THE WORK AREA IF USING A PILOT CAR.
- 3- CONES ARE ALWAYS REQUIRED IN THE UPSTREAM AND DOWNSTREAM TAPERS.
- 4- MOUNT SIGN G20-4 "PILOT CAR FOLLOW ME" AT A CONSPICUOUS POSITION ON THE REAR OF THE PILOT VEHICLE.
- 5- UNLESS APPROVED BY THE ENGINEER, DO NOT INSTALL MORE THAN ONE (1) MILE OF LANE CLOSURE, MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.
- 6- ADVISE RESIDENTS AND BUSINESSES WITHIN THE LANE CLOSURE LIMITS ABOUT METHODS OF SAFE EGRESS AND INGRESS FROM DRIVEWAYS DURING FLAGGING AND PILOT CAR OPERATIONS.

LEGEND

- FLAGGER
- CONE
- PORTABLE SIGN
- DIRECTION OF TRAFFIC FLOW

PROJECT NO.	SHEET NO.	TOTAL NO.
2025CPT.10.02.10041	SOQ 1	

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	BEGIN MP	END MP	1220000000-E	1245000000-E	1260000000-E	1297000000-E	1330000000-E	1523000000-E	1524000000-E	1575000000-E	1704000000-E	1775000000-E	1838000000-E	1840000000-E	2612000000-E	6000000000-E	6071002000-E	6071010000-E
														INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	AGGREGATE SHOULDER BORROW	2" MILLING	INCIDENTAL MILLING	SURFACE COURSE, \$9.5C	LEVELING COURSE, \$9.5C	ASPHALT BINDER FOR PLANT MIX	PATCHING EXISTING PAVEMENT	ASPHALT SURFACE TREATMENT, MATCOAT, #78M STONE	EMULSION FOR ASPHALT SURFACE TREATMENT	MILLED RUMBLE STRIPS (ASPHALT CONCRETE)	6" DRIVEWAYS	TEMPORARY SILT FENCE	FLOCCULANT	WATTLE
														TONS	SMI	TON	SY	SY	TONS	TONS	TONS	TONS	SY	GAL	LF	SY	LF	LB	LF
2025CPT.10.02.10041	Anson	1	US-74 W ROUTE # 20600074004	FROM SR 1846 GRAVEL PLANT RD TO BRIDGE PAVMT JOINT WEST OF SR 1801 Hailey's Ferry Rd MP 3.47 TO MP 5.5Z	1	2	MD	NO	NO	2.1	32-56	3.47	5.57	125	4.20	777		2,394	5,668	525	401	735	39,424	9,860	22,176	18	315	1	315
TOTAL FOR MAP NO. 1														125	4.20	777		2,394	5,668	525	401	735	39,424	9,860	22,176	18	315	1	315
2025CPT.10.02.10041	Anson	2	US-52 N ROUTE # 20000052004	FROM RR BRIDGE TO PVMT JOINT SR 1634 GRASSY ISLAND RD MP 19.5 TO MP 23.1Q	2,3	2	2WU	NO	NO	3.6	25	19.5	23.1	150	7.20	1,331		1,693	6,983	900	526	1,260	55,622	13,910			540	2	540
TOTAL FOR MAP NO. 2														150	7.20	1,331		1,693	6,983	900	526	1,260	55,622	13,910			540	2	540
2025CPT.10.02.10041	Anson	3	US-52 N ROUTE # 20000052004	FROM PVMT JOINT SR 1634 GRASSY ISLAND RD TO PVMT JOINT PAST FRIEHOUSE AVE MP 23.1Q TO MP 23.79	4,5,6	2	2WU	NO	NO	0.69	25-36	23.1	23.79	5	0.62	115	16,103		2,077		138	311	10,658	2,665			104		104
TOTAL FOR MAP NO. 3														5	0.62	115	16,103		2,077		138	311	10,658	2,665			104		104
TOTAL FOR PROJ NO. 2025CPT.10.02.10041														280	12.02	2,223	16,103	4,087	14,728	1,425	1,065	2,306	105,704	26,435	22,176	18	959	3	959
GRAND TOTAL														280	12.02	2,223	16,103	4,087	14,728	1,425	1,065	2,306	105,704	26,435	22,176	18	959	3	959

PROJECT NO.	SHEET NO.	TOTAL NO.
2025CPT.10.02.10041	SOQ 2	

THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNT Y	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	LENGTH	WIDTH	BEGIN MP	END MP	4413000000-E	4457000000-N	4685000000-E		4695000000-E	4700000000-E	4709000000-E	4720000000-E	4725000000-E				4770000000-E	4905100000-N	
												WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	4" X 90 M WHITE THERMO	4" X 90 M YELLOW THERMO	8" X 90 M YELLOW THERMO	12" X 90 M YELLOW THERMO	THERMOPLASTIC PAVEMENT MARKING LINES (24", 90 MILS)	THERMO MSG SCHOOL 90 M	THERMO RT ARROW 90 M	THERMO LT ARROW 90 M	THERMO STR & RT ARROW 90 M	THERMO STR ARROW 90 M	COLD APPLIED PLASTIC PAVEMENT MARKING LINES, TYPE ** (4")	NON-CAST IRON SNOWPLOWABLE PAVEMENT MARKER	
								MI	FT			SF	LS	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	
2025CPT.10.02.10041	Anson	1	US-74 W ROUTE # 20600074004	FROM SR 1846 GRAVEL PLANT RD TO BRIDGE PAVMT JOINT WEST OF SR 1801 Hailey's Ferry Rd MP 3.47 TO MP 5.57	1	2	MD	2.1	32-56	3.47	5.57	235	0.30	15,980	11,088					2	15				206	
TOTAL FOR MAP NO. 1								2.1				235	0.30	15,980	11,088					2	15				206	
2025CPT.10.02.10041	Anson	2	US-52 N ROUTE # 20000052004	FROM RR BRIDGE TO PVMT JOINT SR 1634 GRASSY ISLAND RD MP 19.5 TO MP 23.10	2,3	2	2WU	3.6	25	19.5	23.1	403	0.50	38,214	21,937		74				1	2	2	926.00	248	
TOTAL FOR MAP NO. 2								3.6				403	0.50	38,214	21,937		74				1	2	2		926	248
2025CPT.10.02.10041	Anson	3	US-52 N ROUTE # 20000052004	FROM PVMT JOINT SR 1634 GRASSY ISLAND RD TOPVMT JOINT PAST FRIEHOUSE AVE MP 23.10 TO MP 23.79	4,5,6	2	2WU	0.69	25-36	23.1	23.79	77	0.20	5,795	7,796	54		50	6		4				60	
TOTAL FOR MAP NO. 3								0.69				77	0.200	5,795	7,796	54		50	6		4					60
TOTAL FOR PROJ NO. 2025CPT.10.02.10041								6.39				715	1.000	59,989	40,821	54	74	50	6	2	20	2	2		926	514
GRAND TOTAL								6.39				715	1.000	59,989	40,821	54	74	50	6	2	20	2	2		926	514
														100,810						26						